

**Meeting:** 11/19/2025  
**Applicant:** Logan Higgins Heyoh Architecture  
**Owner:** Logan Higgins Arlington Downs Partnership, LLC  
**District:** Oakwood/Lincoln Park Infill Housing Overlay District

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## Property Information

**Location:** 3547 Oswald St. **Parcel ID:** 69 N E 00401  
**Zoning:** I-MU (Industrial Mixed-Use)  
**Description:** Revisions to multi-family building (8-G-23-IH) and new plat

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## Staff Recommendation

The Board should discuss the front porch design within the historic context and the “intermediate gravel drive and parking” lot.

Along with any necessary revisions or conditions, staff recommends approval of Certificate 9-F-25-IH, subject to the following conditions:

- 1) the final site plan, parking areas, and elevation drawings to meet the standards of the I-MU zone, Article 9.3.I, and any applicable standards of the zoning code and building codes;
  - 2) City Engineering approval of the “intermediate gravel drive and parking” lot, its driveway access, and proposed sidewalks;
  - 3) the “proposed future parking” lot be approved as part of a future application;
  - 4) the final plat to meet the Subdivision Regulations, with major changes to return to the Board;
  - 5) the final site plan to incorporate walkways from the porches to the street or sidewalk required landscaping in the “intermediate” parking area and all elevations, including native or naturalized ornamental or shade trees along Oswald Street, with final selection and placement to meet any applicable standards, including Article 12, and be sent to staff for approval;
  - 6) the brick detailing on the parapets, windows, and foundation to be retained;
  - 7) final material specifications to be sent to staff for approval, with cladding on the concrete block foundation as necessary.
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## Description of Work

Level II Subdivision/Plat, New Primary Structure

Overview: New plat and revisions to 8-G-23-IH, new multi-family building that is part of a multi-phase 8-acre mixed-use development.

Plat: New plat creates two lots by subdividing 1.04 acres of the 8.01-acre existing parcel. Plat reflects the irregular, “C” shape of the site plan for this phase of the project. The property is approximately 300’ wide by 175’ deep, wrapping around and excluding the three existing house lots along Oswald Street. New easements for pedestrian access, no-construction restriction zone, vehicular access, parking, and stormwater are added.

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Site plan: Parking lot access location in relation to the Rider Avenue is now shown as being offset with the intersection, and as a result, exiting vehicles may be restricted to right-turns only. Parking is now provided via an “intermediate gravel drive and parking” area located to the rear (west) of the proposed buildings that is accessed via Oswald Street at the same location as the “proposed future parking.” Porches along Oswald Street are flush with the building, with a front setback that varies from 1’-10” to 6’-4” due to curved lot line, and their steps and walkways connect with the street. Sidewalk is shown within Oswald Street right-of-way, but the applicant has indicated that this may not be installed. This has not been indicated other than removing the note “proposed sidewalks” on the revised plans.

Left (south) building massing: Removal of third-story paneled massing. Increase height of main façade from 26’-7” to 28’-8” and 10” increase in foundation height. Private patios along Oswald Street revised to porches flush with the main massing with a simple metal handrailing and steps projecting to the street on the right side of each unit, screened by a brick knee wall. Removed second-story balconies from Oswald Street elevation. Revised courtyard form and materials, with the addition of a gate, and removal of some windows. Minor modifications to window and door placement and sizes. Conversion of some casement windows to fixed. Removal of one balcony on the west elevation and revisions to balcony structure. Minor modifications to massing pattern and awnings on south elevation. Additional brick detailing on the parapet and on the foundation along Oswald Street.

Central (west) connector massing: Revised form, pattern, height, and parapets of modules, with different materials and placement on east and west elevations. Removal of two street-facing pedestrian entrances, and reduced size and visibility of two remaining openings. Removal of glass block and addition of full-lite walls. Revised window, door, and balcony placement.

Right (north) building massing: Increase height from 27’ to 29’-2”. Conversion of some windows to paired windows. Conversion of some casement windows to fixed. Minor changes to window and door sizes and placement. Additional brick detailing on the parapet and foundation. Private patio of Oswald Street unit has been converted into a front porch similar to on the left (south) massing, with the steps on the left side and three simple supports.

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## Comments

1. This project, 8-G-23-IH, was first heard and postponed in August 2023, and the plans were significantly revised before they returned to the Board in February 2024, where they were approved with the following conditions: “1) Final site plan and parking areas to meet City Engineering standards; 2) Any parking areas fronting Oswald Street to receive landscaping and screening to meet design guidelines; 3) Final drawings to meet standards of the I-MU zone and principal use standards for multi-family dwellings; 4) Specifications for final materials to be submitted to staff for approval.”

The current case for the revised plans was postponed at the September 2025 meeting because the Board did not have a quorum to review it and at the October 2025 meeting to allow the applicant to make revisions to add front porches along Oswald Street, which have been introduced. The November 2025 submission also includes a plat that creates two lots, following the site plan boundary, which was not previously reviewed.

2. The proposed plat creating two lots from the current 8.01-acre lot requires approval from the Board because it does not create 50’ wide lots. The property has an irregular shape and was historically an industrial site, so it does not have a historic residential development pattern. The proposed plat creates two new lots that follow the boundaries of the proposed site plan and existing exterior lot lines. Pedestrian and vehicular access easements have been added to reflect the pedestrian amenities indicated on the site plan. The proposed plat is appropriate. It makes residential development easier on the site, could be subdivided into 50’ lots if the project did not happen, and would not increase the scale of any possible non-residential use of the site.

3. The front setback has been increased slightly from the previous submission to allow for front porches, which are between 1'-10" to 2'-4.5" from the front lot line. The steps project from the building and appear to connect with walkways to terminate at the front lot line. The final site plan should feature walkways from the porch to the street or sidewalk. These revisions reflect the Board's feedback and the design guidelines.

4. Parking is now provided via an "intermediate gravel drive and parking" area shown on the Phase 1.2A Plan (page 22), located to the rear (west) of the proposed buildings, with connections to Oswald Street with the same access as the "proposed future parking." The previous submittal also included a connection from Colonial Avenue. While the intent of this parking area to be temporary, it has to be considered as if it is permanent, meeting all zoning standards, since there is no guarantee if or when it will be removed. The gravel surface is an approvable driving and parking surface, pending approval by City Engineering. The parking is located behind the building and accessed via one lane, which meets design guidelines. With the exception of the proposed access point to Oswald Street discussed below, staff is recommending that the "proposed future parking" lot shown on the Overall Phase Site Plan (page 21) be approved as part of a future application since design details are likely to change based on future submittals.

The driveway location in relation to Rider Avenue has been corrected, showing that it is offset with the intersection. Due to this, City Engineering has indicated that exiting vehicles will be restricted to right-out turns only because of conflicting turn movements at the intersection. The driveway and parking lot drive aisle width on the "proposed future parking" area have been reduced to 26' width from 30' based on the plans submitted for the September 2025 agenda. The driveway curb cut width could be further reduced to 20', but this may not be possible because of the design requirements associated with restricting turn movements. The parking lot may be required to have a 25' setback from Oswald Street since it will serve a mixed-use development and has common frontage on the same block with residentially zoned property (Article 11.3.B).

5. The general landscaping plan has been reintroduced in the current plan set, showing the intent along the parking lot and internal to the site. Landscaping (likely shrubs) has been added along Oswald Street since the previous submission. The placement is appropriate, but landscaping should include native or naturalized ornamental or shade trees. The final site plan should meet the landscaping standards of Article 12 and all other applicable zoning and City Engineering standards, with final selection and design of landscaping to be approved by staff.

6. Guidelines recommend multi-family construction be similar in height to the context. The Board approved the two- and three-story height in 2023, with the third story being a penthouse on the left (south) massing. This request removes the third-story penthouse, resulting in an overall decrease in building height from 34'-5" to 28'-8". The height of the right (north) building massing that faces the rear of existing single-family houses along Colonial Avenue has increased from 26' to 29'-2".

7. Guidelines for multi-family buildings recommend they "continue the architectural rhythm of the block" by "dividing the building into separate sections that are proportionally similar to original houses on the block" and encourage "new apartment buildings be designed in...context with the early architectural features of the neighborhood." This is primarily achieved via the bay window massings. The revisions to the central building massing, particularly the Oswald Street elevation, create modules that are larger in scale than previously proposed. Additional brick detailing has been added to the foundation and parapets of the brick massings, which reflect the historic context.

8. The reintroduction of the porches and steps along Oswald Street, per Board request, increases the building's interaction with the street and follows the recommendations of the design guidelines. The placement of the recessed staircase makes the porch space usable and large enough for seating, and while they are not 8' deep per guideline recommendations, they are similar in scale to the building and other multifamily development approved within the overlay. The removal of the second-story balconies along Oswald Street helps the building align with its single family context.

Guidelines recommend that “porch posts and railings should be like those used in the historic era of the neighborhood’s development.” The plans approved in 2024 included similar front porches, with the steps projecting instead of recessed and placed on different sides of the units. The Board approved the use of simple metal handrailings with thin balusters and brick knee walls next to the steps in 2024, which are also proposed in the revised plans. The metal handrailings do not reflect porches on historic houses in the area; neither do the kneewalls as stepped wing walls were more common flanking front porch steps. Although design guidelines discourage the use of wrought iron porch posts and railings, wrought iron railings are common in the area, and metal railings with more visual interest would be more compatible with the historic context, as would the use of stepped wing walls flanking the steps. The Board should discuss if revisions should be made to the porch railings and brick knee walls to better align the more modern building with the historic context.

9. The reduction of the number and size of the pedestrian openings on the central building massing makes the pedestrian amenities less visible and accessible from Oswald Street. The revised design of the central building massing on the alley-facing west elevation provides transparency that activates the pedestrian openings and breaks the massing into smaller, varied modules, but these tactics are not employed on the street-facing east elevation. The promenade and pedestrian openings are pedestrian entrances at the north of the overall development that provide internal public access to the proposed commercial buildings. The revisions to the east (Oswald Street) elevation obscure access to the development from the public.

9. Guidelines recommend that window and door styles be similar to historic houses on the block, with similar ratio of solid to void. The Board approved the use of full-lite doors, single-lite casement windows, and single-lite fixed windows in 2024.

There have been minor revisions to the window sizes, operation, and placement, including the conversion of some casement windows to fixed windows. The applicant converted some of the fixed windows to casement windows since the September 2025 plans, particularly along the Oswald Street elevation, which makes this primary elevation more compatible with the context. These changes meet the design guidelines.

10. The left (south) building massing no longer features the third-story shed roof massing of the penthouses, which reduces the overall height of the building. The revisions to the modules in the central building massing result in a different roof pattern that is generally similar to what was previously proposed and is compatible with its modern form. The design incorporates flat roofs with parapets, similar to historic apartment buildings in Knoxville, and additional detailing has been added to the cornice of the left (south) and right (north) building massings which benefit the overall design.

11. Brick veneer and vertical siding are the primary exterior cladding materials. The brick veneer meets the design guidelines, but the guidelines discourage the use of vertical siding. The Board approved the use of vertical siding on the central building massing in 2024, but the modifications to the module pattern on the east (Oswald Street) elevation make the material more visible from the right-of-way. Most of the foundation will be clad in brick veneer. The concrete block foundation on the central building massing should be screened by an exterior cladding material, parge-coated, or clad in stucco. Final product specifications should be sent to staff for approval.

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## Applicable Design Guidelines

Heart of Knoxville Infill Housing Design Guidelines

### 1. Front Yards

- Consistent front yard space should be created along the street with the setback of a new house matching the older houses on the block.
- A walkway should be provided from the sidewalk or street to the front door. Along grid streets, the walk should be perpendicular to the street.

- Healthy trees that are outside the building footprint should be preserved. The root area should be marked and protected during construction.

## 2. Housing Orientation

- New housing should be proportional to the dimensions of the lot and other houses on the block.
- On corner lots, side yard setbacks should be handled traditionally (that is, closer to the side street). The zoning requirement to treat corner lots as having two frontages should not apply in Heart of Knoxville neighborhoods.
- Side yard setbacks should be similar to older houses on the block, keeping the rhythm of spacing between houses consistent.

## 3. Alleys, Parking, and Services

- Parking should not be in front yards.
- Alley access should be used for garage or parking pad locations. On level ground, pea gravel or similar material may be used as a parking pad off alleys.
- On streets without alleys, garages or parking pads should be at least 20 feet behind the front façade of the infill house with access limited to one lane between the street and the front façade.
- Garages which are perpendicular to the alley should be about 18 feet from the center line of the alley pavement, allowing a comfortable turning radius for a driver to enter a garage.
- Alley-oriented parking pads, garbage collection points, and utility boxes should be screened with a combination of landscaping and fencing.
- On those streets which have alleys, driveways should not be permitted from the front of the house.
- On corner lots, a driveway to the garage may be provided off the side street.

## 4. Scale, Mass, and Foundation Height

- The front elevation should be designed to be similar in scale to other houses along the street.
- The front façade of new houses should be about the same width as original houses on the block.
- New foundations should be about the same height as the original houses in the neighborhood.
- If greater height is to be created (with new construction or an addition), that portion of the house should be located toward the side or rear of the property.

## 5. Porches and Stoops

- Porches should be part of the housing design in those neighborhoods where porches were commonplace.
- Porches should be proportional to original porches on the block, extending about 8-12 feet toward the street from the habitable portion of the house.
- Porches should extend into the front yard setback, if necessary, to maintain consistency with similarly sited porches along the street.
- Porch posts and railings should be like those used in the historic era of the neighborhood's development. Wrought iron columns and other materials that were not used in the early 1900's should not be used.
- Small stoops centered on entry and no more than 5 feet deep are appropriate on blocks where porches were not traditional.

## 6. Windows and Doors

- When constructing new houses, the window and door styles should be similar to the original or historic houses on the block.
- To respect the privacy of adjacent properties, consider the placement of side windows and doors.
- The windows and doors on the front facade of an infill house should be located in similar proportion and position as the original houses on the block.
- Attention should be paid to window placement and the ratio of solid (the wall) to void (the window and door openings).
- Contemporary windows such as "picture windows" should not be used in pre-World War II neighborhoods.

## 7. Roof Shapes and Materials

- New roofs should be designed to have a similar pitch to original housing on the block
- More complex roofs, such as hipped roofs and dormers, should be part of new housing designs when such forms were historically used on the block.
- Darker shades of shingle were often used and should be chosen in roofing houses in Heart of Knoxville neighborhoods.

## 8. Siding Material

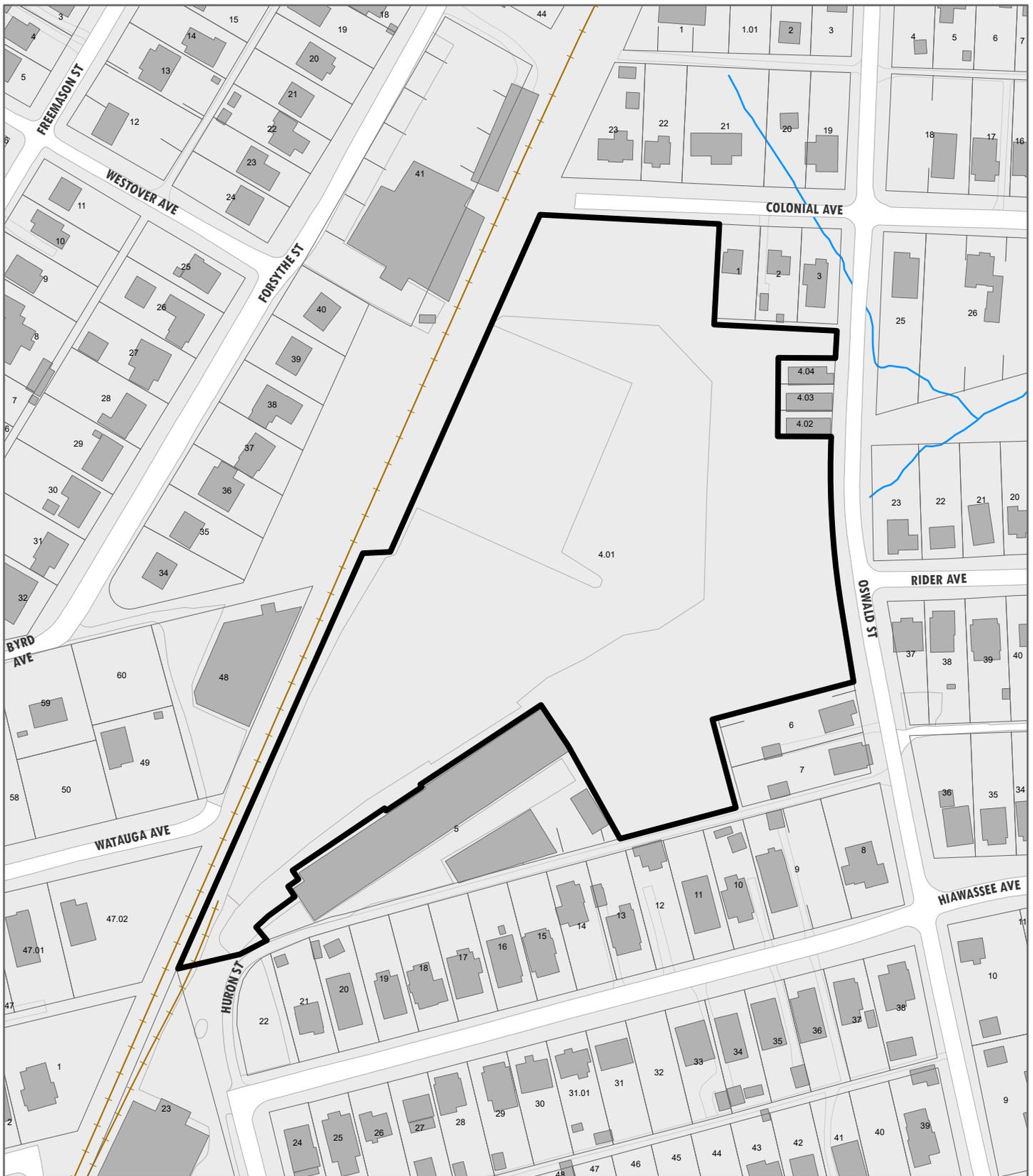
- Clapboard-like materials (such as cement fiberboard) should be used in constructing new housing where painted wood siding was traditionally used.
- Brick, wood shingle, and other less common material may be appropriate in some older neighborhoods, particularly those with a mix of architectural styles.
- Faced stone, vertical siding, and other non-historic materials should not be used in building new houses. In 1930-1950 era neighborhoods, faced stone may be appropriate (see Section 12).

## 11. Landscape and Other Considerations

- One native or naturalized shade tree should be planted in the front and rear yards of in fill lots with 25 feet or more in depth to front of house.

## 10. Multi-Unit Housing

- Multi-unit housing (where permitted by zoning) should have similar front yard space to that of the traditional single-family houses along the street.
- In zoning districts where multi-unit housing is permitted, the height of the new housing should be similar to the original houses along the street.
- Multi-unit housing should be designed to continue the architectural rhythm of the block. In addition to the same build-to line, porches, bays and breaks in the front façade should be created to mimic the look of older homes when looking down the block. This should be done by dividing the building into separate sections that are proportionally similar to original houses on the block.
- Parking should be provided behind apartments with access from the alley.



**DESIGN  
REVIEW  
BOARD**

**9-F-25-IH**  
**APPLICATION FOR CERTIFICATE OF APPROPRIATENESS**

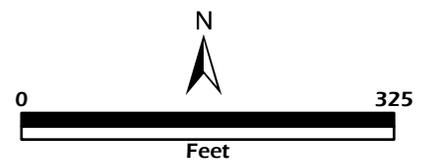


3547 Oswald St.  
Oakwood/Lincoln Park Infill Housing Overlay  
District

Original Print Date: 9/8/2025  
Knoxville - Knox County Planning - Design Review Board

Revised:

Petitioner: Logan Higgins Heyoh  
Architecture





# DESIGN REVIEW REQUEST

- DOWNTOWN DESIGN (DK)
- HISTORIC ZONING (H)
- INFILL HOUSING (IH)

Heyoh Design and Development

Applicant

8-29-25

September 17, 2025

9-F-25-IH

Date Filed

Meeting Date (if applicable)

File Number(s)

## CORRESPONDENCE

All correspondence related to this application should be directed to the approved contact listed below.

- Owner
- Contractor
- Engineer
- Architect/Landscape Architect

Logan Higgins		Heyoh Design + Development		
Name		Company		
133 S Gay Street		Knoxville	TN	37902
Address		City	State	Zip
865.236.0430		Logan@heyohdesign.com		
Phone		Email		

## CURRENT PROPERTY INFO

Arlington Downs Partnership, LLC		133 S Gay Street Ste C		865.236.0430
Owner Name (if different from applicant)		Owner Address		Owner Phone
3527 Oswald St		069NE00401		
Property Address		Parcel ID		
Oakwood Lincoln Park		I-MU		
Neighborhood		Zoning		

## AUTHORIZATION

		Malynda Wollert	8-29-25
Staff Signature		Please Print	Date
		Logan Higgins	8-29-25
Applicant Signature		Please Print	Date

# REQUEST

## DOWNTOWN DESIGN

**Level 1:**

- Signs     Alteration of an existing building/structure

**Level 2:**

- Addition to an existing building/structure

**Level 3:**

- Construction of new building/structure     Site design, parking, plazas, landscape

**See required Downtown Design attachment for more details.**

Brief description of work: \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

## HISTORIC ZONING

**Level 1:**

- Signs     Routine repair of siding, windows, roof, or other features, in-kind; Installation of gutters, storm windows/doors

**Level 2:**

- Major repair, removal, or replacement of architectural elements or materials     Additions and accessory structures

**Level 3:**

- Construction of a new primary building

**Level 4:**

- Relocation of a contributing structure     Demolition of a contributing structure

**See required Historic Zoning attachment for more details.**

Brief description of work: \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

## INFILL HOUSING

**Level 1:**

- Driveways, parking pads, access point, garages or similar facilities     Subdivisions

**Level 2:**

- Additions visible from the primary street     Changes to porches visible from the primary street

**Level 3:**

- New primary structure  
      Site built     Modular     Multi-Sectional

**See required Infill Housing attachment for more details.**

Brief description of work: New phase of the Arlington Downs new urbanist community for a new building with 14 townhouse units.  
 (Updated packet from August 2023)  
 \_\_\_\_\_  
 \_\_\_\_\_

## STAFF USE ONLY

**ATTACHMENTS**

- Downtown Design Checklist  
 Historic Zoning Design Checklist  
 Infill Housing Design Checklist

**ADDITIONAL REQUIREMENTS**

- Property Owners / Option Holders

**Level 1:** \$50 • **Level 2:** \$100 • **Level 3:** \$250 • **Level 4:** \$500

<b>FEE 1:</b>	100	<b>TOTAL:</b>  100.00 Paid 8/29/25 DD
<b>FEE 2:</b>		
<b>FEE 3:</b>		

CERTIFICATE OF OWNERSHIP AND GENERAL DEDICATION

(I, WE, THE UNDERSIGNED OWNER(S) OF THE PROPERTY SHOWN HEREIN, HEREBY ADOPT THIS AS MY, OUR) PLAN OF SUBDIVISION AND DEDICATE THE STREETS AS SHOWN TO THE PUBLIC USE FOREVER AND HEREBY CERTIFY THAT I (AM WE ARE) THE OWNER(S) IN FEE SIMPLE OF THE PROPERTY, AND AS PROPERTY OWNER(S) HAVE AN UNRESTRICTED RIGHT TO DEDICATE RIGHT-OF-WAY AND/OR GRANT EASEMENT AS SHOWN ON THIS PLAN.

ARLINGTON DOWNS PARTNERSHIP, LLC DATE
STATE OF TENNESSEE COUNTY OF KNOX ON THIS DAY OF 2020
BEFORE ME PERSONALLY APPEARED
TO ME KNOWN TO BE THE PERSON DESCRIBED IN, AND WHO EXECUTED THE FOREGOING INSTRUMENT, AND ACKNOWLEDGED THAT HE EXECUTED THE SAME AS HIS FREE ACT AND DEED. WITNESS MY HAND AND NOTARIAL SEAL, THIS DAY AND YEAR ABOVE.

OWNER CERTIFICATION FOR PUBLIC SEWER AND WATER SERVICE - MINOR SUBDIVISIONS
(I, WE) THE UNDERSIGNED OWNER(S) OF THE PROPERTY SHOWN HEREIN UNDERSTAND THAT IT IS OUR RESPONSIBILITY TO VERIFY WITH THE UTILITY PROVIDER THE AVAILABILITY OF PUBLIC SEWER AND WATER SYSTEMS IN THE VICINITY OF THE LOT(S) AND TO PAY FOR THE INSTALLATION OF THE REQUIRED CONNECTIONS.

CERTIFICATION OF FINAL PLAT - ALL INDICATED MARKERS, MONUMENTS AND BENCHMARKS SET
I HEREBY CERTIFY THAT I AM A REGISTERED LAND SURVEYOR LICENSED TO PRACTICE SURVEYING UNDER THE LAWS OF THE STATE OF TENNESSEE. I FURTHER CERTIFY THAT THIS PLAN AND ACCOMPANYING DRAWINGS, DOCUMENTS, AND STATEMENTS CONFORM TO THE BEST OF MY KNOWLEDGE, TO ALL APPLICABLE PROVISIONS OF THE KNOXVILLE-KNOX COUNTY SUBDIVISION REGULATIONS EXCEPT AS HAS BEEN ITEMIZED, DESCRIBED AND JUSTIFIED IN A REPORT FILED WITH THE PLANNING COMMISSION, OR FOR VARIANCES AND WAIVERS WHICH HAVE BEEN APPROVED AS IDENTIFIED ON THE FINAL PLAT, THE INDICATED PERMANENT REFERENCE MARKERS AND MONUMENTS, BENCHMARKS AND PROPERTY MONUMENTS WERE IN PLACE ON THE DAY OF 2020.

CERTIFICATION OF CATEGORY AND ACCURACY OF SURVEY
I HEREBY CERTIFY THAT THIS IS A CATEGORY 1 SURVEY AND THE RATIO OF PRECISION OF THE UNADJUSTED SURVEY IS NOT LESS THAN 1/10,000 AS SHOWN HEREON AND THAT SAID SURVEY WAS PREPARED IN COMPLIANCE WITH CURRENT EDITION OF THE RULES OF TENNESSEE STATE BOARD OF EXAMINERS FOR LAND SURVEYORS-STANDARDS OF PRACTICE.

CERTIFICATION OF APPROVAL OF PUBLIC SANITARY SEWER SYSTEM - MINOR SUBDIVISIONS
THIS IS TO CERTIFY THAT THE SUBDIVISION SHOWN HEREON IS APPROVED SUBJECT TO THE INSTALLATION OF A PUBLIC WATER SYSTEM, AND THAT TREATMENT FACILITIES, AND THAT SUCH INSTALLATION SHALL BE IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS. IT IS THE RESPONSIBILITY OF THE PROPERTY OWNER TO VERIFY WITH THE UTILITY PROVIDER THE AVAILABILITY OF SANITARY SEWERS IN THE VICINITY OF THE LOT(S) AND TO PAY FOR THE INSTALLATION OF THE REQUIRED CONNECTIONS.

CERTIFICATION OF APPROVAL OF PUBLIC WATER SYSTEM - MINOR SUBDIVISIONS
THIS IS TO CERTIFY THAT THE SUBDIVISION SHOWN HEREON IS APPROVED SUBJECT TO THE INSTALLATION OF A PUBLIC WATER SYSTEM, AND THAT SUCH INSTALLATION SHALL BE IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS. IT IS THE RESPONSIBILITY OF THE PROPERTY OWNER TO VERIFY WITH THE UTILITY PROVIDER THE AVAILABILITY OF WATER SYSTEM IN THE VICINITY OF THE LOT(S) AND TO PAY FOR THE INSTALLATION OF THE REQUIRED CONNECTIONS.

CITY OF KNOXVILLE DEPARTMENT OF ENGINEERING
THE KNOXVILLE DEPARTMENT OF ENGINEERING HEREBY APPROVES THIS PLAN ON THIS DAY OF 2020.

ENGINEERING DIRECTOR
ADDRESSING DEPARTMENT CERTIFICATION
I, THE UNDERSIGNED, HEREBY CERTIFY THAT THE SUBDIVISION NAME AND ALL STREET NAMES CONFORM TO THE KNOXVILLE OR KNOX COUNTY STREET NAMING AND ADDRESSING ORDINANCE, THE ADDRESSING GUIDELINES AND PROCEDURES, AND THESE REGULATIONS.

ADDRESSING REPRESENTATIVE DATE
ZONING
ZONING SHOWN ON OFFICIAL MAP
ZONING REPRESENTATIVE DATE
TAXES AND ASSESSMENTS
THIS IS TO CERTIFY THAT ALL PROPERTY TAXES AND ASSESSMENTS DUE ON THIS PROPERTY HAVE BEEN PAID.

CITY TAX CLERK DATE
KNOX COUNTY TRUSTEE DATE
OWNER CERTIFICATION ON RIGHT-OF-WAY ENCROACHMENT
(I, WE) the undersigned owner(s) of the property shown herein understand that the City of Knoxville is not approving the project of any encroachments into any right-of-ways(s) as shown hereon, on that (I, we) accept full responsibility for any resulting consequences thereof.

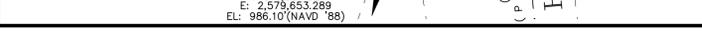
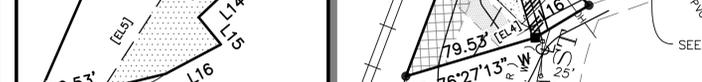
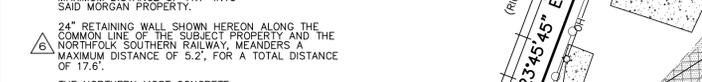
OWNER(S) PRINTED NAME SIGNATURE DATE
PLANNING STAFF CERTIFICATION OF APPROVAL FOR RECORDING - FINAL PLAT
THIS IS TO CERTIFY THAT THE SUBDIVISION PLAT SHOWN HEREON HAS BEEN FOUND TO COMPLY WITH THE SUBDIVISION REGULATIONS OF KNOXVILLE AND KNOX COUNTY AND WITH EXISTING OFFICIAL PLANS, WITH THE EXCEPTION OF ANY VARIANCES AND WAIVERS NOTED ON THIS PLAN, AND THAT THE RECORD PLAT IS HEREBY APPROVED FOR RECORDING IN THE OFFICE OF THE KNOX COUNTY REGISTER OF DEEDS, PURSUANT TO SECTION 13-3-405 OF TENNESSEE CODE ANNOTATED THE APPROVAL OF THIS PLAT BY THE PLANNING COMMISSION SHALL NOT BE DEEMED TO CONSTITUTE OR EFFECT AN ACCEPTANCE BY THE CITY OF KNOXVILLE OR KNOX COUNTY OF THE DEDICATION OF ANY STREET OR OTHER GROUND UPON THE PLAT.

PLANNING STAFF MEMBER DATE

NOTES

- 1) RECORD INFORMATION
OWNER: ARLINGTON DOWNS PARTNERSHIP, LLC
DEED: 20210150031374 (CORRECTION DEED)
SUBDIVISION: FINAL PLAT OF ARLINGTON DOWNS, FORMERLY PLATTED AS THE 'FINAL PLAT OF THE JONATHAN E. BAILEY PROPERTY'
PLAT: 20211100038991
2) THE PURPOSE OF THIS PLAT IS TO:
RESUBDIVIDE LOT 1R, CREATING 2 NEW LOTS
CREATE PEDESTRIAN ACCESS EASEMENT AND NO-CONSTRUCTION RESTRICTION ZONE IN FAVOR OF BOTH LOTS 1R-1 AND 1R-2.
CREATE A PEDESTRIAN ACCESS EASEMENT FOR FUTURE BREEZEWAY. THIS EASEMENT AREA IS LIMITED TO A CLEARANCE OF TEN (10) FEET VERTICAL ABOVE GRADE.
CREATE AN EASEMENT FOR VEHICULAR ACCESS, PARKING, AND PEDESTRIAN ACCESS IN FAVOR OF BOTH LOTS 1R-1 AND 1R-2.
CREATE A TWENTY FIVE (25) FOOT WIDE SHARED ACCESS EASEMENT IN FAVOR OF LOT 1R-2 AND THE LOGAN HIGGINS PROPERTY (PARCEL 069 NE 005).
REMOVE THE EXISTING 20' WATER QUALITY EASEMENT AND ITS ASSOCIATED TRAVERSABLE ACCESS EASEMENT (CREATED BY PLAT 201605040063637) - FACILITY NO LONGER IN USE.
SHOW THE INTENT TO REMOVE THE EXISTING 15' INGRESS & EGRESS EASEMENT CREATED BY WARRANTY BOOK 1030 PAGE 29 AND SHOWN ON PLAT 201605040063637 AND 20211100038991 - THE LOCATION OF THIS WAS EASEMENT WAS APPROXIMATE ONLY AND PROPERTY THAT BENEFITS FROM THIS EASEMENT (SHERMAN CONCRETE PIPE MACHINERY, INC. PROPERTY) AND THE PROPERTY THAT IS ENCUMBERED BY THIS EASEMENT (THOMAS E. ARP) BOTH APPPEAR WITHIN THE CHAIN OF TITLE TO THE CURRENT OWNER.
CREATE AN EASEMENT FOR AN EXISTING STORM WATER 'B' PIPE THAT TRANSECTS THE SOUTHWEST CORNER OF THE PROPERTY.
'KNOXVILLE GRID NORTH' BASED ON A BEARING OF SOUTH 76°31'45" WEST FROM CONTROL STATION No. 0478 (N:613,661,192 E:2,581,002,807) TO CONTROL STATION No. 0479 (N:613,337,927 E:2,579,653,289). HORIZONTAL DATUM BEING NAD83(2011) (FAD11) (MAD11) EPOCH 2010.00. DISTANCES HAVE NOT BEEN REDUCED TO GRID DISTANCES - DISTANCES ARE GROUND DISTANCES. COMBED SCALE FACTOR: 0.99994265.
IRON PINS AT ALL CORNERS, UNLESS DESIGNATED OTHERWISE.
SUBJECT PROPERTY LIES WITHIN ZONE 'X' (OTHER AREAS), AS DESIGNATED BY FEDERAL EMERGENCY MANAGEMENT AGENCY MAP NO. 100-10-0001, WHICH HAS AN EFFECTIVE DATE OF 05 AUGUST 2013, WHICH MAKES UP PART OF THE NATIONAL FLOOD INSURANCE RATE MAP FOR THE COMMUNITY IN WHICH SAID PREMISES IS SITUATED.
THE SURVEYOR IS NOT RESPONSIBLE FOR ANY DETERMINATION OR LOCATION OF ANY UNDERGROUND CONDITION NOT VISIBLE AND INCLUDING BUT NOT LIMITED TO, SOILS, GEOLOGICAL CONDITIONS, PHYSICAL DEVICES AND PIPELINES OR BURIED CABLES. THE SURVEYOR SHALL NOT BE RESPONSIBLE FOR ANY LIABILITY THAT MAY ARISE OUT OF THE MAKING OF OR FAILURE TO MAKE SUCH DETERMINATION OR LOCATION OF ANY SUBSURFACE CONDITION, FOR UNDERGROUND UTILITY LOCATION CALL 811.
SUBJECT PROPERTY IS ZONED 'I-MU'; 'INDUSTRIAL MIXED USE' AND IS SUBJECT TO FOLLOWING SETBACK REQUIREMENTS:
FRONT: BUILD TO ZONE: 0' TO 25'
INTERIOR SIDE: NONE, UNLESS ABUTTING RESIDENTIAL DISTRICT, THEN 15'
CORNER SIDE: BUILD TO ZONE: 0' TO 25'
REAR: 15'
PROPERTY IS ALSO LOCATED WITHIN THE INFILL HOUSING OVERLAY.
THE ACCOMPANYING SURVEY WAS MADE ON THE GROUND AND CORRECTLY SHOWS THE LOCATION OF ALL BUILDINGS, STRUCTURES AND OTHER IMPROVEMENTS SITUATED ON THE ABOVE PREMISES; THERE ARE NO UNDISCOVERED ENCROACHMENTS ON THE SUBJECT PROPERTY OR UPON ADJACENT LAND ABUTTING SAID PROPERTY EXCEPT AS SHOWN HEREON AND WAS MADE IN ACCORDANCE WITH LAWS AND/OR MINIMUM STANDARDS OF THE STATE OF TENNESSEE.
SURVEY WAS PREPARED UTILIZING THE MOST RECENT RECORD DOCUMENTS FOR THE SUBJECT AND SURROUNDING PROPERTIES. SURVEY IS SUBJECT TO ANY FINDING REVEALED BY AN ACCURATE TITLE SEARCH. NO TITLE SEARCH WAS PROVIDED TO THIS SURVEYOR FOR THE COMPILED OF THIS SURVEY.
SUBJECT PROPERTY IS SUBJECT TO A UTILITY LINE EASEMENT AS DESCRIBED IN DEED BOOK 215, PAGE 402 OF SAID REGISTER'S OFFICE. (NO WELL DEFINED AND NO PHYSICAL EVIDENCE OF SAME OBSERVED AT THE TIME OF THIS SURVEY)
A STANDARD UTILITY & DRAINAGE EASEMENT SITUATED ALONG ALL LOT LINES IN A SUBDIVISION, SHALL BE DEDICATED TO THE PUBLIC AND TO APPROPRIATE UTILITY AGENCIES AS FOLLOWS:
10 FEET WIDE INSIDE ALL EXTERIOR LOT LINES ADJOINING STREETS AND PRIVATE RIGHT-OF-WAY (INCLUDING JOINT PERMANENT EASEMENTS)
5 FEET WIDE ALONG BOTH SIDES OF ALL INTERIOR LOT LINES AND ALONG ALL OTHER EXTERIOR LOT LINES, GIVEN THE PROPERTY ZONING, STANDARD UTILITY AND DRAINAGE EASEMENTS ARE NOT REQUIRED ALONG SIDE LOT LINES.
THE PROPERTY IS CURRENTLY ZONED (I-MU) WHICH PER THE CURRENT ZONING ORDINANCE ALLOWS A ZERO (0) FOOT MINIMUM BUILDING SETBACK ALONG THE FRONT, AND SIDE BOUNDARY LINES UNLESS ABUTTING RESIDENTIAL PER SUBDIVISION REGULATION 3.11-A.1. THE STANDARD UTILITY AND DRAINAGE EASEMENTS DEDICATION IS NOT REQUIRED AND THIS AND PLAT DOES NOT DEDICATE SAID SETBACKS ALONG THOSE LOT LINES UNLESS ABUTTING RESIDENTIAL. ADDITIONAL STANDARD UTILITY AND DRAINAGE EASEMENTS THAT ARE REQUIRED SHALL BE DEDICATED AS STATED / SHOWN HEREON, UNLESS NOTED OTHERWISE.
NO EASEMENTS WERE FOUND OF RECORD FOR THE UNDERGROUND SEWER LINE THAT CROSS THE SUBJECT PROPERTY, HOWEVER PRESCRIPTIVE RIGHTS MOST LIKELY EXISTS FOR SAID UNDERGROUND SEWER LINE.
SOME OF THE INFORMATION REGARDING THE 10' STORM EASEMENT SHOWN HEREON WAS TAKEN FROM CITY STORM ASBUILT DRAWINGS PROVIDED TO THIS SURVEYOR BY THE CITY OF KNOXVILLE DEPARTMENT OF ENGINEERING.
THIS PLAT INDICATES ONE OR MORE BOUNDARY LINE ENCROACHMENTS, THESE ARE NOT ADDRESSED BY PLANNING, AND ARE THE RESPONSIBILITY OF THE PROPERTY OWNERS TO RESOLVE.
ENCROACHMENT NOTES
THIS PLAT INDICATES THE FOLLOWING PROPERTY BOUNDARY ENCROACHMENTS. THEY HAVE NOT BEEN ADDRESSED BY PLAT REVIEWING AGENCIES, OWNERS ARE RESPONSIBLE FOR RESOLVING PROPERTY BOUNDARY ENCROACHMENTS.
FENCE LINE SHOWN HEREON ALONG THE SOUTHERLY R.O.W. OF COLONIAL AVENUE LIES WHOLLY ON THE SUBJECT PROPERTY.
FENCE LINE SHOWN HEREON ALONG THE COMMON LINE OF THE SUBJECT PROPERTY & LOTS 8, 9 & 9 OF BLOCK 12, REVISED MAP OF PORTION OF PIEDMONT PLACE, MEASURES A MAXIMUM DISTANCE OF 2.3' FROM SAID LINE FOR A TOTAL DISTANCE OF 146.6'.
FENCE LINE SHOWN HEREON ALONG THE WESTERLY R.O.W. OF OSWALD STREET MEASURES A MAXIMUM DISTANCE OF 6.0' FROM SAID R.O.W. FOR A TOTAL DISTANCE OF 356.1'.
FENCE LINE SHOWN HEREON ALONG THE NORTHERLY R.O.W. OF THIS MOST SOUTHERN 15' ALLEY A MAXIMUM DISTANCE OF 2.0' FROM SAID R.O.W. FOR A TOTAL DISTANCE OF 144'.
FENCE LINE SHOWN HEREON AT A COMMON SOUTHERLY CORNER OF THE SUBJECT PROPERTY & LOGAN HIGGINS PROPERTY, EXTENDS A MAXIMUM DISTANCE OF 11.7' INTO SAID MORGAN PROPERTY.
24" RETAINING WALL SHOWN HEREON ALONG THE COMMON LINE OF THE SUBJECT PROPERTY AND THE NORTHERLY R.O.W. OF OSWALD STREET MEASURES A MAXIMUM DISTANCE OF 5.2', FOR A TOTAL DISTANCE OF 17.6'.
THE NORTHERN MOST CONCRETE PAD SHOWN HEREON, EXTENDS 6.3' INTO THE NORTHERLY R.O.W. OF OSWALD STREET, FOR A TOTAL DISTANCE OF 142.8'.
THE BUILDING SITUATED NEAR THE COMMON LINE OF THE SUBJECT PROPERTY AND THE LOGAN HIGGINS PROPERTY LIES WHOLLY ON SAID MORGAN PROPERTY, EXCEPT FOR ONE CORNER AND CMU WALL ON THE SOUTHWEST END OF THE BUILDING - SHOWN IN DETAIL "A".

ENCROACHMENT NOTES (continued)
25' SHARED ACCESS EASEMENT IN FAVOR OF LOT 1R-2 AND THE LOGAN HIGGINS PROPERTY (PARCEL 069 NE 005)
DETAIL "B" 1"=50'

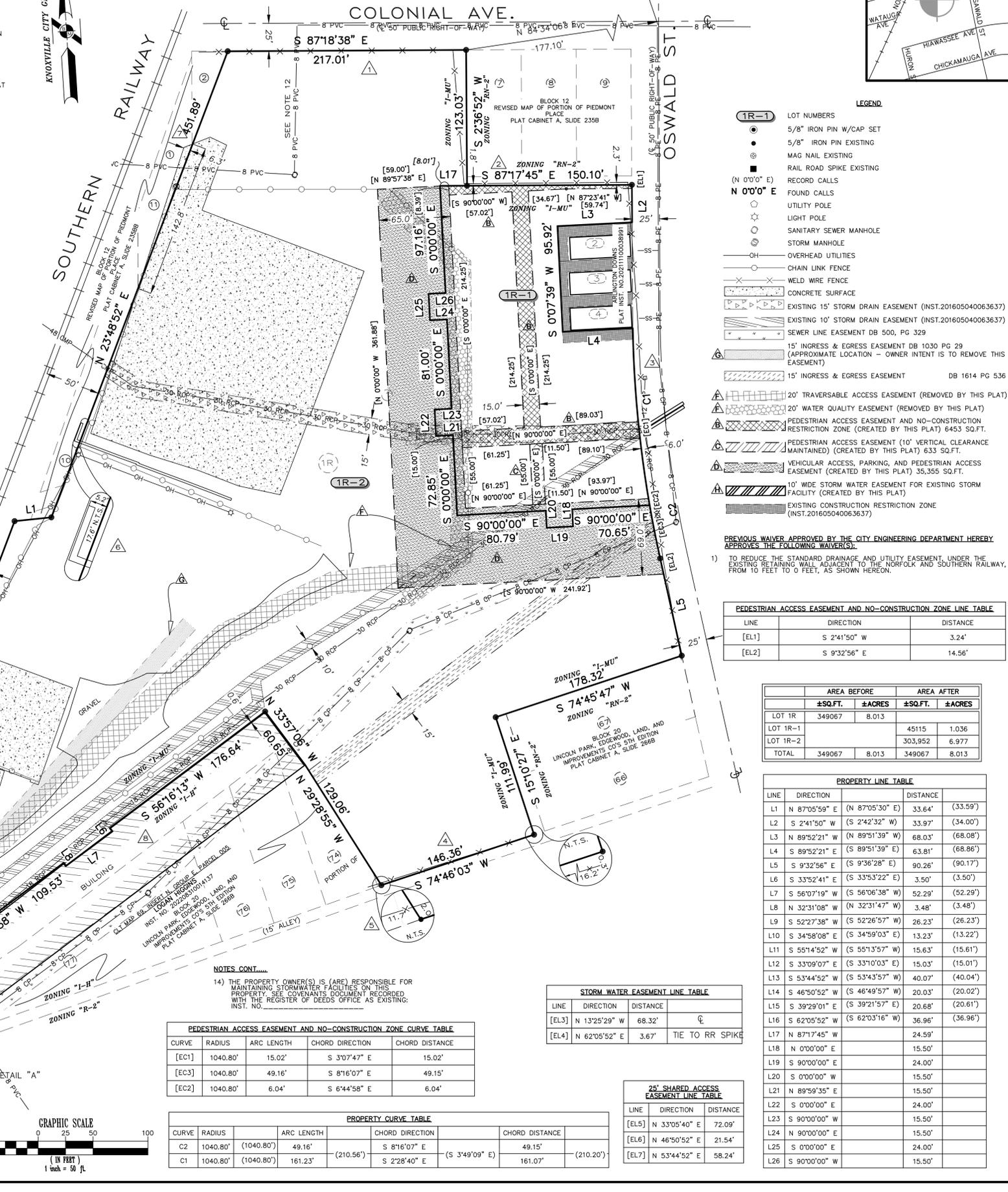


OWNER

ARLINGTON DOWNS PARTNERSHIP, LLC
133 S. GAY ST., STE C
KNOXVILLE, TN 37902
(423)-502-4120

Resubdivison Plat of Lot 1R of Arlington Downs

BEING LOCATED ON QLT MAP 069 INSERT N GROUP E PARCELS 004.01 SITUATED WITHIN THE SECOND CIVIL DISTRICT OF KNOX COUNTY, TENNESSEE WITHIN THE EIGHTEENTH CITY WARD OF THE CITY OF KNOXVILLE, TENNESSEE CITY BLOCK 18300



PLANNING FILE NUMBER 8-DDD-25



- LEGEND
1R-1 LOT NUMBERS
5/8" IRON PIN W/CAP SET
5/8" IRON PIN EXISTING
MAG NAIL EXISTING
RAIL ROAD SPIKE EXISTING
RECORD CALLS
FOUND CALLS
UTILITY POLE
LIGHT POLE
SANITARY SEWER MANHOLE
STORM MANHOLE
OVERHEAD UTILITIES
CHAIN LINK FENCE
WELD WIRE FENCE
CONCRETE SURFACE
EXISTING 15' STORM DRAIN EASEMENT (INST.201605040063637)
EXISTING 10' STORM DRAIN EASEMENT (INST.201605040063637)
SEWER LINE EASEMENT DB 500, PG 329
15' INGRESS & EGRESS EASEMENT DB 1030 PG 29 (APPROXIMATE LOCATION - OWNER INTENT IS TO REMOVE THIS EASEMENT)
15' INGRESS & EGRESS EASEMENT DB 1614 PG 536
20' TRAVERSABLE ACCESS EASEMENT (REMOVED BY THIS PLAT)
20' WATER QUALITY EASEMENT (REMOVED BY THIS PLAT)
PEDESTRIAN ACCESS EASEMENT AND NO-CONSTRUCTION RESTRICTION ZONE (CREATED BY THIS PLAT) 6453 SQ.FT.
PEDESTRIAN ACCESS EASEMENT (10' VERTICAL CLEARANCE MAINTAINED) (CREATED BY THIS PLAT) 633 SQ.FT.
VEHICULAR ACCESS, PARKING, AND PEDESTRIAN ACCESS EASEMENT (CREATED BY THIS PLAT) 35,355 SQ.FT.
10' WIDE STORM WATER EASEMENT FOR EXISTING STORM FACILITY (CREATED BY THIS PLAT)
EXISTING CONSTRUCTION RESTRICTION ZONE (INST.201605040063637)

PREVIOUS WAIVER APPROVED BY THE CITY ENGINEERING DEPARTMENT HEREBY APPROVES THE FOLLOWING WAIVER(S):
1) TO REDUCE THE STANDARD DRAINAGE AND UTILITY EASEMENT, UNDER THE EXISTING RETAINING WALL ADJACENT TO THE NORFOLK AND SOUTHERN RAILWAY, FROM 10 FEET TO 0 FEET, AS SHOWN HEREON.

Table with 3 columns: LINE, DIRECTION, DISTANCE. Contains data for PEDESTRIAN ACCESS EASEMENT AND NO-CONSTRUCTION ZONE LINE TABLE.

Table with 4 columns: AREA BEFORE, AREA AFTER, ±SQ.FT., ±ACRES. Contains data for LOT 1R, LOT 1R-1, LOT 1R-2, and TOTAL.

Table with 3 columns: LINE, DIRECTION, DISTANCE. Contains data for PROPERTY LINE TABLE.

Table with 3 columns: LINE, DIRECTION, DISTANCE. Contains data for STORM WATER EASEMENT LINE TABLE.

Table with 3 columns: LINE, DIRECTION, DISTANCE. Contains data for 25' SHARED ACCESS EASEMENT LINE TABLE.

Table with 5 columns: CURVE, RADIUS, ARC LENGTH, CHORD DIRECTION, CHORD DISTANCE. Contains data for PEDESTRIAN ACCESS EASEMENT AND NO-CONSTRUCTION ZONE CURVE TABLE.

Table with 5 columns: CURVE, RADIUS, ARC LENGTH, CHORD DIRECTION, CHORD DISTANCE. Contains data for PROPERTY CURVE TABLE.

Vertical sidebar containing: BENCHMARK ASSOCIATES, INC. Land Planners & Land Surveyors; Resubdivison Plat of Lot 1R of Arlington Downs; SITE ADDRESS: 10308 Hardin Valley Road, Knoxville, TN 37917; Phone: (865) 692-4090; Fax: (865) 692-4091; DATE: 10/31/2025; SCALE: 1"=50'; DRAWN BY: EP; FILE NAME: 21215-C3D; BM PROJECT NO.: 21-215; FINAL PLAT SHEET 1 of 1.



UPDATED RENDERING

# ARLINGTON DOWNS 1.2

3527 OSWALD STREET  
INFILL HOUSING REVIEW

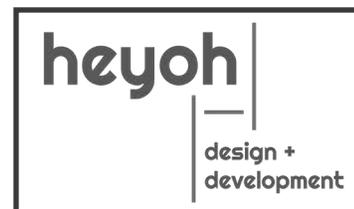


## INDEX

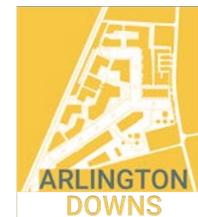
- 03 LOCATION
- 04 OVERVIEW
- 05 SITE HISTORY
- 06 PRECEDENTS
- 08 DEVELOPMENT PLAN
- 09 3D VIEWS
- 11 DESIGN GUIDELINES
- 21 SITE PLAN
- 22 ELEVATIONS
- 24 ADDENDUM

**Disclaimer: all renderings represent design intent; they do not necessarily represent any massing or building form design changes.**

This application has been prepared to seek approval of the project from Knoxville's Design Review Board under Infill Housing Review.



Heyohdesign.com  
865.236.0430  
Admin@heyohdesign.com



Arlington Downs  
Partnership LLC



# OVERVIEW

Arlington Downs, an 8-acre mixed-use development in Knoxville's Lincoln Park and Arlington neighborhoods, is set to become a dynamic and open community space. The project is being developed in multiple phases over several years.

The development's diverse structures include single-family, two-family, multi-family, and commercial buildings. Additionally, nearly two acres of land will be reserved for public parks with walking paths and a versatile common area for community events.

The initial phase focused on constructing single-family houses along the perimeter, with the first three homes already built. This next phase will be a multifamily structure with 14 townhouse units occupying nearly 17,000 square feet.

Arlington Downs signifies a transformational undertaking that seeks to create an inviting urban landscape and foster a vibrant, cohesive community.



Rendering for design intent only.

**FUTURE BUILDINGS**

**EXISTING HOUSES**

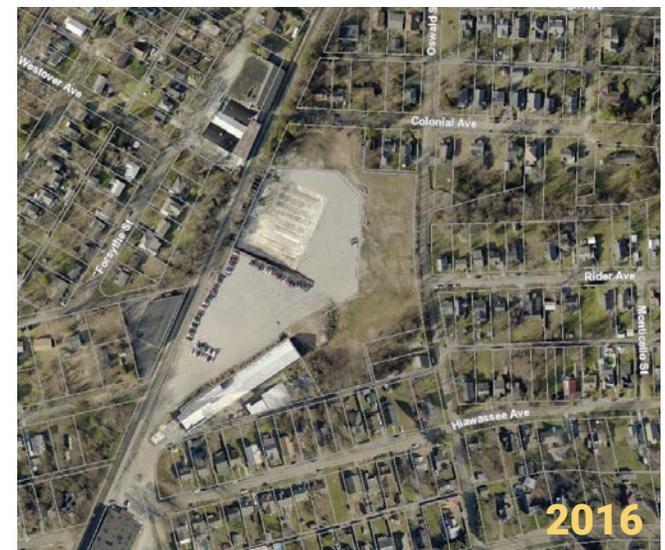
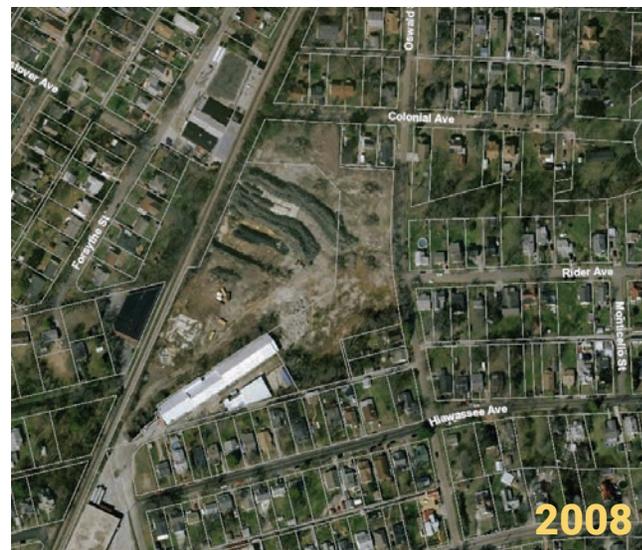
**SUBJECT PROPERTY**

This element has changed since our last DRB presentation. In an effort to curb the housing shortage and better engage with the wider neighborhood with better pedestrian access, we think it would be best for this location to be multi-unit with a common sidewalk connecting back to Oswald St.

# SITE HISTORY

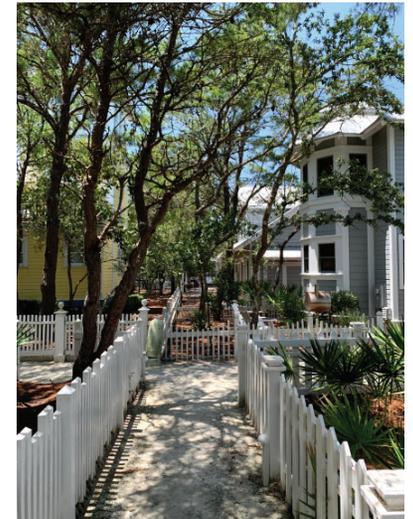
This property has never been developed for residential use. For most of the 20th century, the property was used as a concrete pipe manufacturing facility. By the early 2000's the main buildings were gone and the vacant land became the site of a tire grinding operation until a fire in 2008.

For almost a decade after that, the property sat vacant, until being used as an impound lot in 2015. That use didn't last long and it became vacant once again. Despite all of these industrial uses, environmental reports show a clean bill of health, safe for residences and in 2021, it was purchased by a group of investors and neighbors to develop a new-urbanist community. Unlike most infill projects, this is an entire 2-block industrial site that could be the site of anything from factory to housing, it will have several new houses and multiple commercial buildings. In some sense, it will establish its own context but it intends to follow the guidelines to the extent that they are relevant.



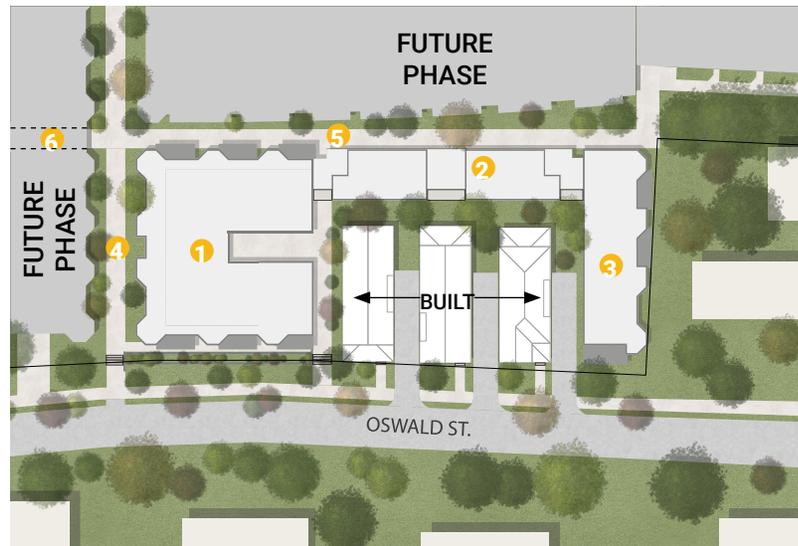
# PRECEDENT FOR URBAN DESIGN

## SEASIDE, FLORIDA



# PRECEDENTS FOR BUILDING DESIGN





- LEGEND**
- 1. 2-STORIES TOWNHOUSES ALONG THE PROMENADE WITH PRIVATE ENTRANCES
  - 2. 2-STORY TOWNHOUSES W/ PRIVATE ENTRANCES AND BALCONIES
  - 3. 2-STORY TOWNHOUSES WITH SEMI-PRIVATE PATH TO OSWALD ST.
  - 4. PROMENADE
  - 5. PEDESTRIAN PATH
  - 6. TUNNEL



Rendering for design intent only



Rendering for design intent only



Rendering for design intent only

- MASTER PLAN LEGEND**
- 1. SINGLE FAMILY DETACHED (BUILT)
  - 2. MULTI-FAMILY W/ PARKING BELOW (FUTURE)
  - 3. MIXED-USE COMMERCIAL (FUTURE)
  - 4. BOUTIQUE HOTEL (FUTURE)
  - 5. MULTI-FAMILY RESIDENTIAL (FUTURE)
  - 6. PUBLIC POCKET PARK (FUTURE)
  - 7. MAKER SPACE (FUTURE)
  - 8. BREWERY (FUTURE)



Previous design rendering; changes noted in white

FUTURE BUILDINGS



Previous design rendering; changes noted in white

FUTURE BUILDINGS



FUTURE BUILDING

UPDATED RENDERING

# KNOXVILLE INFILL HOUSING REVIEW GUIDELINES

## EXCERPT FROM HEART OF KNOXVILLE INFILL HOUSING DESIGN GUIDELINES:

### INTRODUCTION

*“The purposes of these guidelines are to re-establish the architectural character of those historically valuable properties with **new housing that is architecturally compatible; to foster neighborhood stability; to recreate more pedestrian-oriented streets; and to meet a wide range of housing needs.** These guidelines should be used to understand the major elements of architectural compatibility and promote housing designs that are usable and economically achievable. **These guidelines have been created to apply to areas where there are no historic or neighborhood conservation zoning overlays or Traditional Neighborhood Development district zoning.** Those areas already have specific guidelines for infill and vacant lot development; the guidelines for those districts are available through the Metropolitan Planning Commission.”*

In an effort to explain the new infill project, we have thoroughly gone through the Infill Housing Review Guidelines and noted on the following pages how we have appropriately responded to each guideline.



Previous design rendering; changes noted in white

# 1. FRONT YARDS

Front yard space was consistent from house to house with porches being located about the same distance from the street. Although there is variation in some blocks, a twenty-five foot setback to the front door is very common. Lawns and an occasional shade tree were found on virtually all lots.

GUIDELINES	APPLICATION
Consistent front yard space should be created along the street with the setback of a new house matching the older houses on the block.	<b>The front face of the new building aligns with the existing three houses that are directly beside it.</b>
When several infill houses are sited, porches and the habitable portion of each house should be about the same distance from the street as the original houses.	<b>The building face aligns with the adjacent houses.</b>
A walkway should be provided from the sidewalk or street to the front door. Along grid streets, the walk should be perpendicular to the street.	<b>All units have a paved walkway from the front door that leads to the street, pedestrian paths, and/or the common parking area.</b>
Fences that are constructed of traditional materials, such as picket fencing, may be used to define the front yard. Chain linked, masonry, wide boards and other contemporary fencing should be used only in backyards.	<b>N/A - Front yard fences are not being proposed.</b>
Healthy trees that are outside the building footprint should be preserved. The root area should be marked and protected during construction.	<b>There are no existing trees to be preserved. But new trees will be planted as part of the tree preservation ordinance.</b>



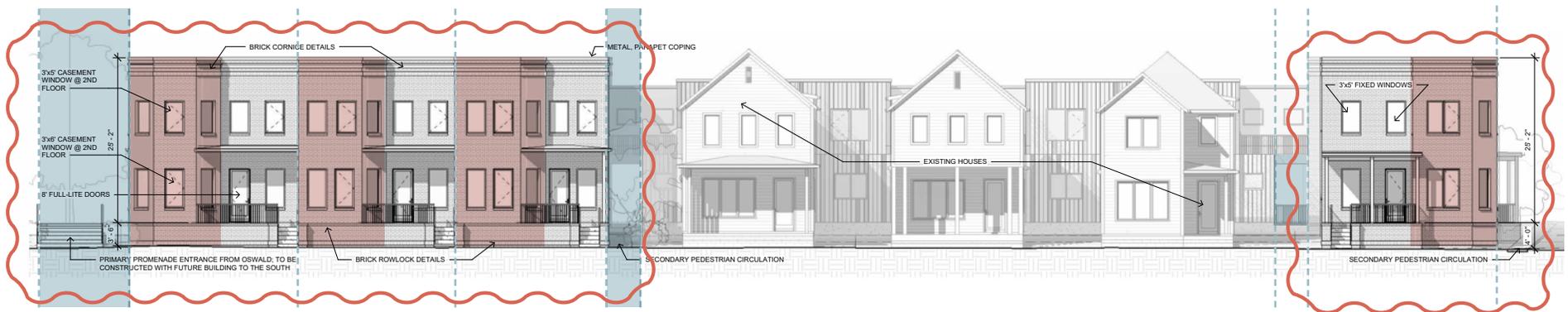
## 2. HOUSE ORIENTATION + SIDE YARDS

The typical city lot prior to 1930 was 50 feet wide. This dimension led to the development of houses which were relatively narrow and had substantial depth. Craftsman style homes are good examples of this characteristic. Because of this characteristic, side yards were relatively narrow.

GUIDELINES	APPLICATION
New housing should be proportional to the dimensions of the lot and other houses on the block.	<b>The projecting bays on the front facade derive their width from the existing houses to provide consistent proportions.</b>
On corner lots, side yard setbacks should be handled traditionally (that is, closer to the side street). The zoning requirement to treat corner lots as having two frontages should not apply in "Heart of Knoxville" neighborhoods.	N/A
Side yard setbacks should be similar to older houses on the block, keeping the rhythm of spacing between houses consistent.	<b>A similar sideyard setback is provided between the adjacent house and new building to match the condition of the existing houses.</b>
On lots greater than 50' in width, consider re-creating the original lot size.	<b>This particular lot has never been used for residential, and therefore there are no original lot sizes to re-create.</b>



Previous design rendering; changes noted in white



### 3. ALLEYS, PARKING, AND SERVICES

Alleys should serve two significant purposes: (1) accommodation of such services as utilities and garbage collection, and (2) access to off-street parking including garages and parking pads. A large proportion of the “Heart of Knoxville” neighborhoods have alleys. Unfortunately, such standards as setbacks for garages, types of materials for parking pads and encouragement of alleys for off-street parking are lacking. With infill development, the use of alleys for parking access is necessary.

#### GUIDELINES

Parking should not be in front yards.

Alley access should be used for garage or parking pad locations. On level ground, pea gravel or similar material may be used as a parking pad off alleys.

On streets without alleys, garages or parking pads should be at least 20 feet behind the front façade of the infill house with access limited to one lane between the street and the front facade.

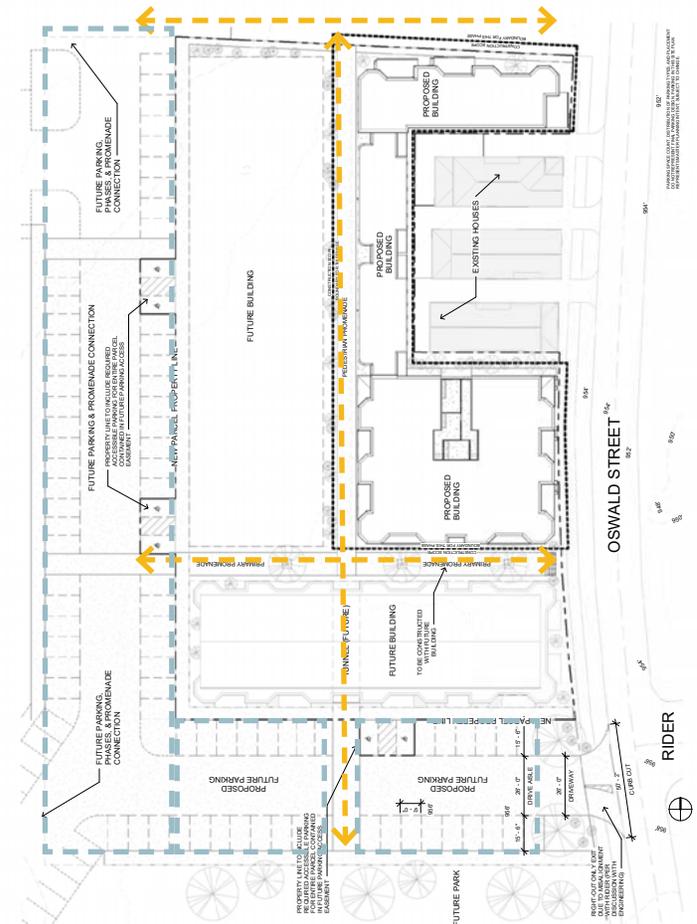
Garages which are perpendicular to the alley should be about 18 feet from the center line of the alley pavement, allowing a comfortable turning radius for a driver to enter a garage.

Alley-oriented parking pads, garbage collection points, and utility boxes should be screened with a combination of landscaping and fencing.

On those streets which have alleys, driveways should not be permitted from the front of the house.

On corner lots, a driveway to the garage may be provided off the side street.

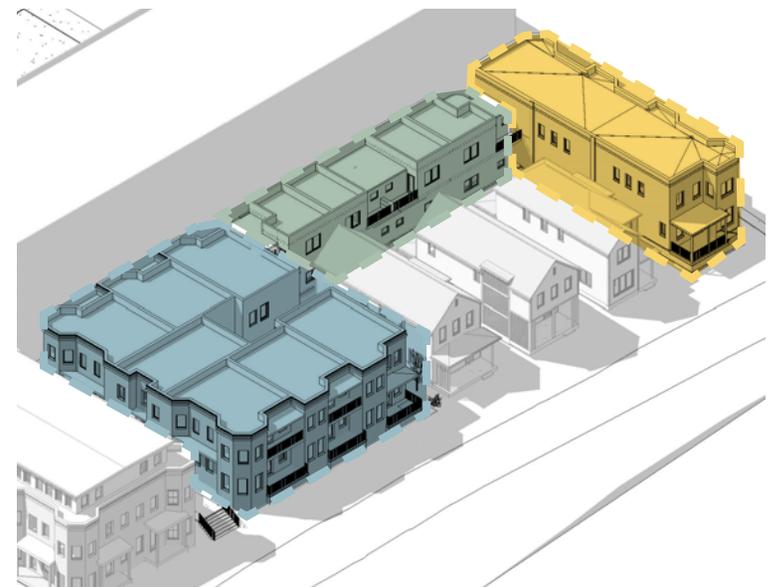
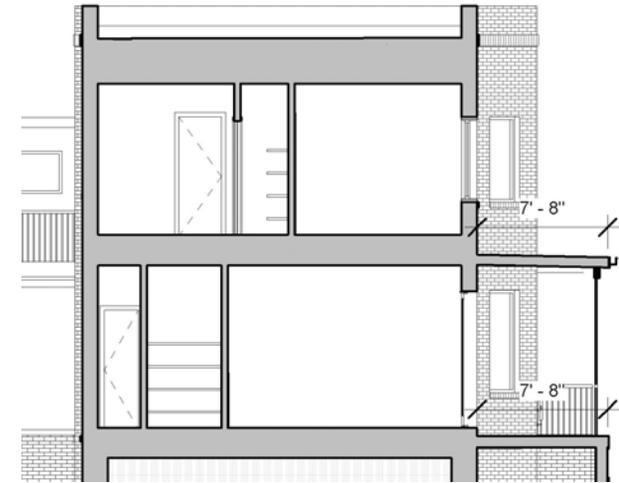
**These guidelines have been followed as applicable. Alleys are provided for pedestrian use only with parking and services provided elsewhere on the site.**



## 4. SCALE, MASS, + FOUNDATION HEIGHT

The scale of early homes was generally consistent from one house to another; especially foundation heights, proportions of first floor elevations, and sizes and shapes of roofs. Sometimes, a one and one-half story house might be found next to a two story house but the essential elements of similar foundation height and façade characteristics created homes that tried to look tall and resulted in architectural compatibility. When a house is built on slab with a low pitch next to a traditional older house, the proportions of the two houses clash, resulting in an absence of architectural harmony. The following principles are critical in maintaining historic and property values.

GUIDELINES	APPLICATION
The front elevation should be designed to be similar in scale to other houses along the street.	<b>We are proposing multi-family but with projecting bays to break up the facade into similar proportions of the adjacent houses.</b>
The front façade of new houses should be about the same width as original houses on the block.	
If extensions or bays were typically part of the neighborhood's historic house design, such elements should be incorporated into infill housing.	<b>Projecting bays are incorporated into the facade design.</b>
New foundations should be about the same height as the original houses in the neighborhood.	<b>Foundation heights are similar to adjacent houses on the site.</b>
If greater height is to be created (with new construction or an addition), that portion of the house should be located toward the side or rear of the property.	
<b>The massing is broken down into three distinct volumes with the large and small brick volumes on the ends being "connected" by the smaller townhouse volume along the pedestrian alley. All three volumes have distinct architectural expressions to further break down the scale.</b>	
<b>The volumes derive their dimensions from those of the adjacent houses.</b>	



# 5. PORCHES + STOOPS

*Porches and stoops were incorporated into most house designs in Knoxville prior to 1900, providing a comfortable place to enjoy a summer breeze or to talk with family and neighbors. Later, front porches became less prominent as such styles as Dutch Colonial, Tudor Revival and Ranch houses became popular. Some houses only had small stoops.*

## GUIDELINES

Porches should be part of the housing design in those neighborhoods where porches were commonplace.

Porches should be proportional to original porches on the block, extending about 8-12 feet toward the street from the habitable portion of the house.

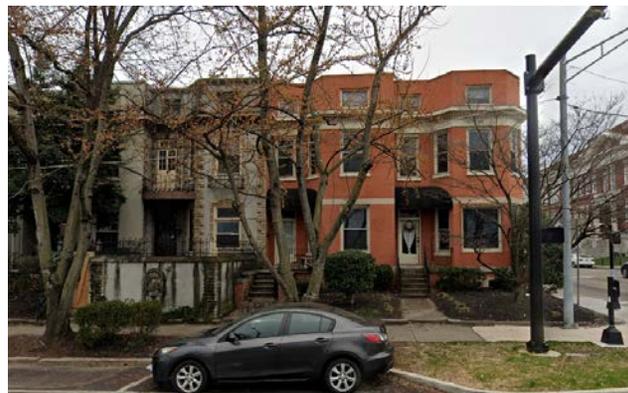
Porches should extend into the front yard setback, if necessary, to maintain consistency with similarly sited porches along the street.

Porch posts and railings should be like those used in the historic era of the neighborhood's development. Wrought iron, "antebellum" columns and other materials that were not used in the early 1900's should not be used.

Small stoops centered on entry and no more than 5 feet deep are appropriate on blocks where porches were not traditional.

The addition of a porch to a Ranch-style house may be acceptable in some 1930-1950 era neighborhoods.

**These guidelines have been followed as applicable. All dwelling units facing the street have small porches or stoops more similar to historic townhouses in the area.**



< SIMILAR STOOP AND PORCH PROPORTIONS TO PROPOSED STOOPS

## 6. WINDOWS + DOORS

Every architectural style also has certain distinguishing window shapes and location. For instance, the windows of Victorian-era houses are narrow and tall. Craftsman houses are broader with a multiple paned sash over a one pane sash. When an infill housing design is selected, the windows should be similar in scale and design to those of other houses on the block.

### GUIDELINES

When constructing new houses, the window and door styles should be similar to the original or historic houses on the block.

To respect the privacy of adjacent properties, consider the placement of side windows and doors.

The windows and doors on the front facade of an infill house should be located in similar proportion and position as the original houses on the block.

Attention should be paid to window placement and the ratio of solid (the wall) to void (the window and door openings).

Contemporary windows such as "picture windows" should not be used in pre-World War II neighborhoods.

**These guidelines have been followed as applicable. The proposed windows will be a combination of metal casement and fixed windows which are tall and narrow to match the predominant proportion in the area. This window style is less commonly used in the area but this is also an example of advances in technology and allowing for contemporary elements. We are also proposing similar metal doors with full glass to maximize daylighting. This is the same approach that was previously taken on the previously approved houses.**



METAL CASEMENT +  
FIXED WINDOWS



METAL DOORS WITH  
FULL GLASS



Rendering for design intent only

# 7. ROOF SHAPES + MATERIALS

*Steeper roof pitches and relatively darker shingles were common to most historic homes and are a basic consideration in new construction. Less pitch is common in Ranch styles, popularized after World War II.*

GUIDELINES	APPLICATION
New roofs should be designed to have a similar pitch to original housing on the block	<b>Since this lot has always been an industrial site, we are proposing flat roof construction similar to other nearby structures along the railroad.</b>
More complex roofs, such as hipped roofs and dormers, should be part of new housing designs when such forms were historically used on the block.	<b>Houses never existed on this site which is the majority of the block.</b>
Darker shades of shingle were often used and should be chosen in roofing houses in Heart of Knoxville neighborhoods.	<b>The roofing material will not be seen from the street, so we are proposing a light colored membrane roof for greater energy efficiency.</b>
In some 1930-1950 era neighborhoods that have a mix of Ranch-style houses, it may be appropriate to change the roof to add a half-story.	<b>N/A</b>



# 8. SIDING MATERIALS

*Clapboard and brick were the most common siding. Houses in some neighborhoods, like Oakwood, were almost totally constructed with clapboard. The exterior materials of new construction should be like that of the neighborhood's older or historic architecture.*

## GUIDELINES

Clapboard-like materials (such as cement fiberboard) should be used in constructing new housing where painted wood siding was traditionally used.

Brick, wood shingle, and other less common material may be appropriate in some older neighborhoods, particularly those with a mix of architectural styles.

Faced stone, vertical siding, and other non-historic materials should not be used in building new houses. In 1930-1950 era neighborhoods, faced stone may be appropriate.

Sheds, garages, and other outbuildings can be constructed of vertical siding or other more economical materials.

**These guidelines have been followed as applicable. Brick is proposed on the most visible portions of the building from the street to resemble historic brick townhouses in the area. Vertical fiber cement panels are proposed for the part of the building along the alley and the interior courtyard.**



DARK BRICK



METAL OR FIBER CEMENT PANELS, VERTICAL



LIGHT BRICK

# 10. MULTI-UNIT HOUSING

Following World War II, many single-family neighborhoods were rezoned to permit apartments. This was done under an urban development theory that the highest density housing should be close to the central business district. The results have been mixed. In some instances the design of multiunit buildings are completely out of context to older neighborhoods with apartment buildings looking like they should have been part of suburbia. In places where multi-unit housing is permitted by zoning, it is essential to neighborhood stability that new apartment buildings be designed in scale and context with the early architectural features of the neighborhood.

GUIDELINES	APPLICATION
Multi-unit housing (where permitted by zoning) should have similar front yard space to that of the traditional single family houses along the street.	<b>The front facade aligns with the facades of the existing houses along the block with similar amount of front yard space.</b>
In zoning districts where multi-unit housing is permitted, the height of the new housing should be similar to the original houses along the street.	<b>The primary roof elevation is consistent with the adjacent houses. It appears slightly higher because of the higher ground elevation.</b>
Multi-unit housing should be designed to continue the architectural rhythm of the block. In addition to the same "build-to line," porches, bays and breaks in the front façade should be created to mimic the look of older homes when looking down the block. This should be done by dividing the building into separate sections that are proportionally similar to original houses on the block.	<b>The projecting bays on the front facade derive their width from the existing houses to provide consistent proportions and rhythm when looking down the block.</b>
Parking should be provided behind apartments with access from the alley.	<b>We are proposing common parking for residents and visitors throughout the development. This phase of the project will include a parking lot between the building and future public park.</b>
Landscaping, including shade trees, should be planted in both front and back yards.	<b>Pedestrian alley to be heavily landscaped with as many trees as feasible to comply with urban forestry.</b>



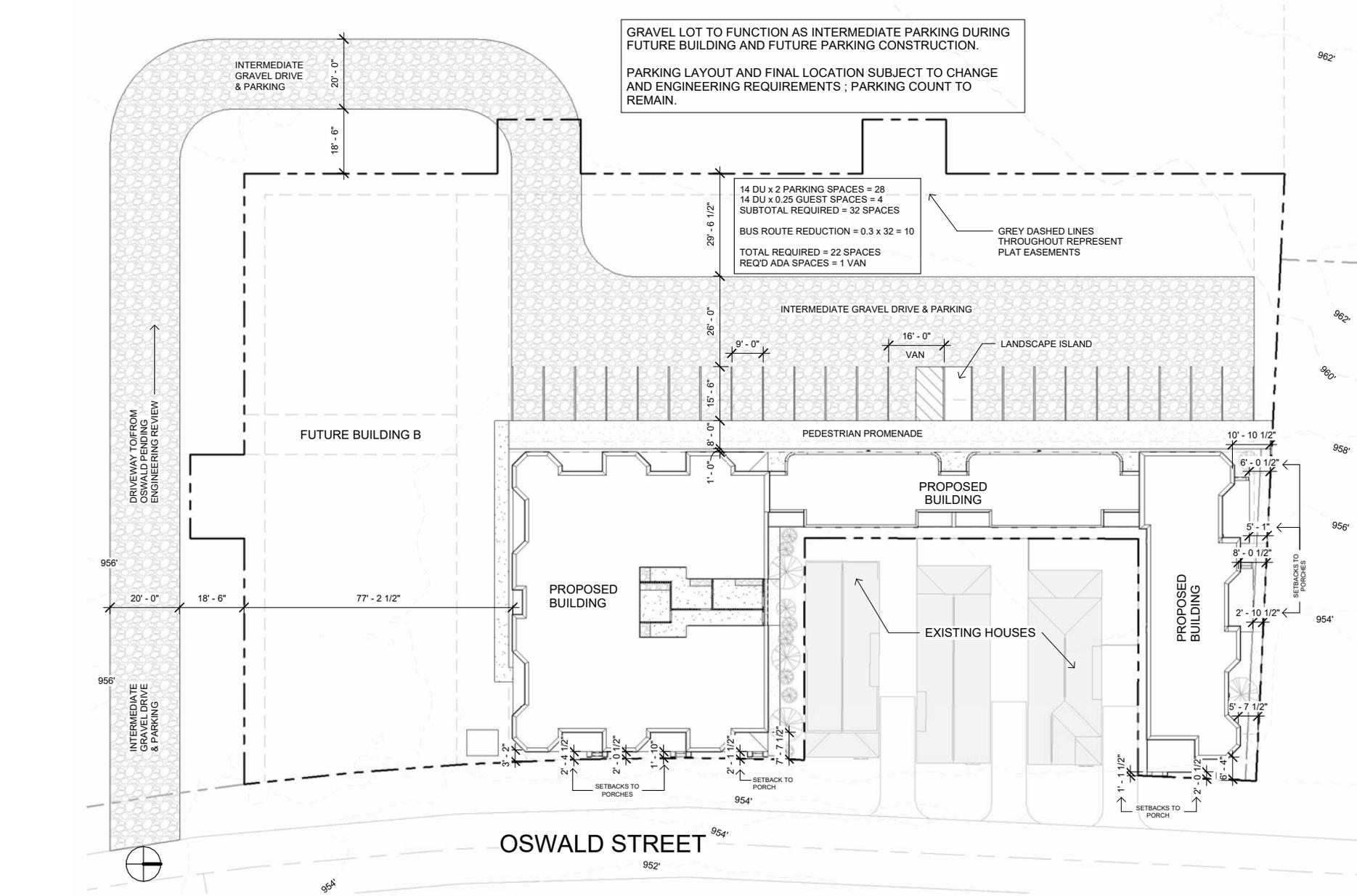


GRAVEL LOT TO FUNCTION AS INTERMEDIATE PARKING DURING FUTURE BUILDING AND FUTURE PARKING CONSTRUCTION.

PARKING LAYOUT AND FINAL LOCATION SUBJECT TO CHANGE AND ENGINEERING REQUIREMENTS ; PARKING COUNT TO REMAIN.

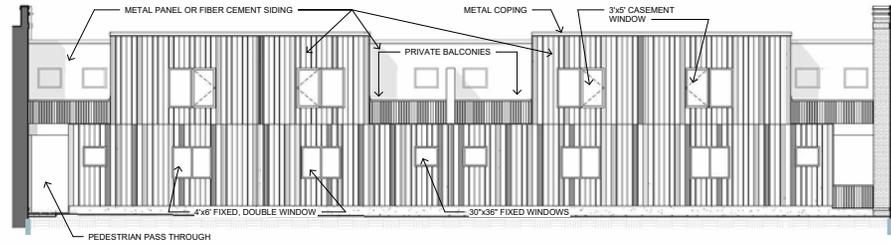
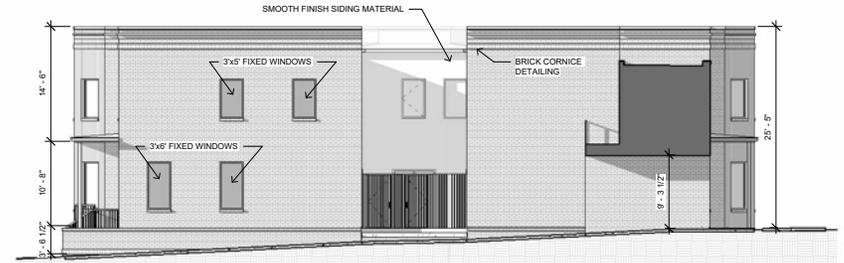
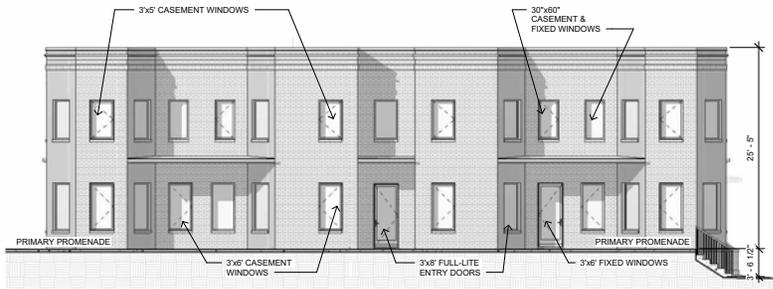
14 DU x 2 PARKING SPACES = 28  
 14 DU x 0.25 GUEST SPACES = 4  
 SUBTOTAL REQUIRED = 32 SPACES  
 BUS ROUTE REDUCTION = 0.3 x 32 = 10  
 TOTAL REQUIRED = 22 SPACES  
 REQ'D ADA SPACES = 1 VAN

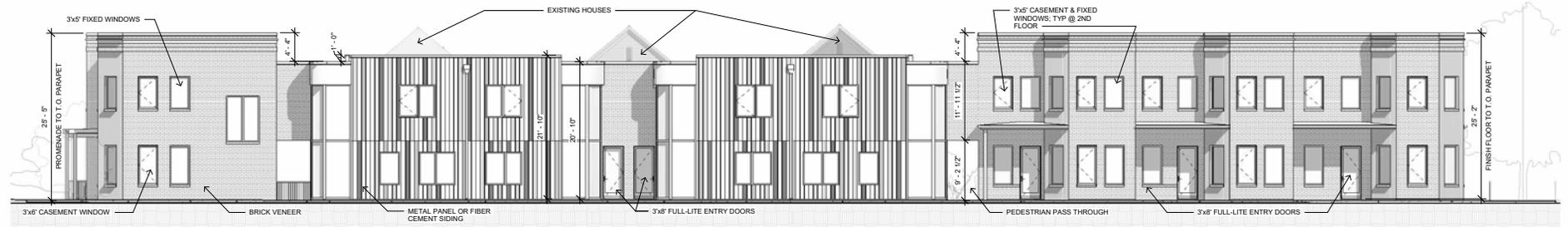
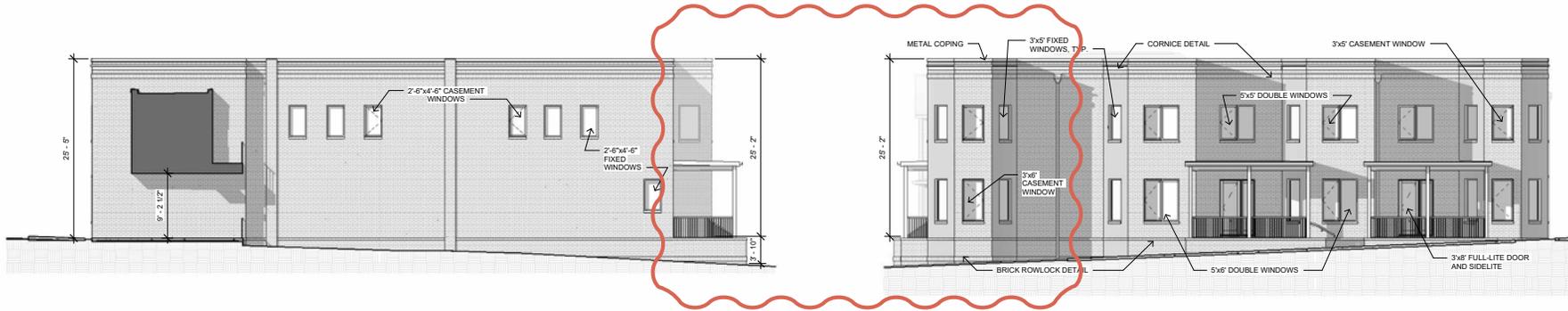
GREY DASHED LINES THROUGHOUT REPRESENT PLAT EASEMENTS



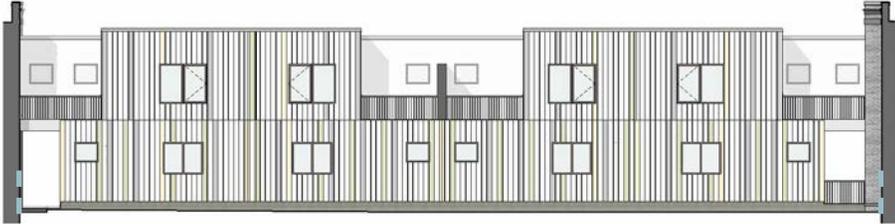
PHASE 1.2A  
 PLAN

22





# ADDENDUM



# ADDENDUM



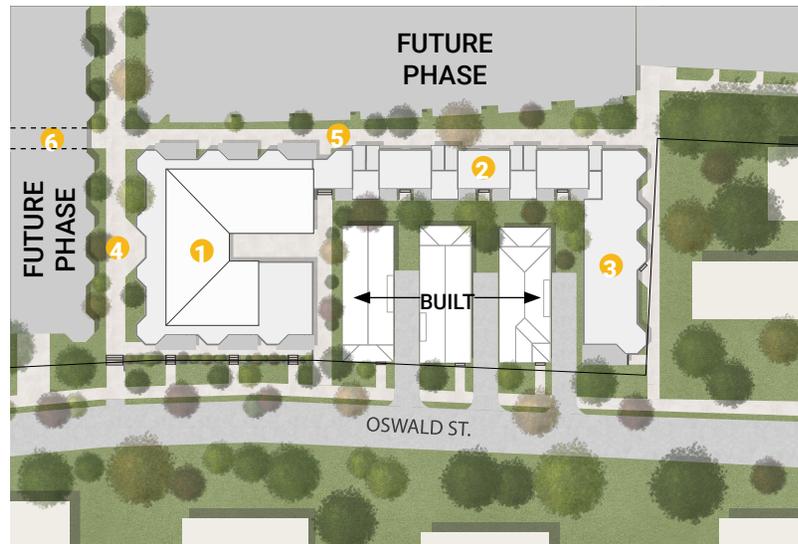
0' 6' 12' 24'

Previous plans



# ARLINGTON DOWNS 1.2

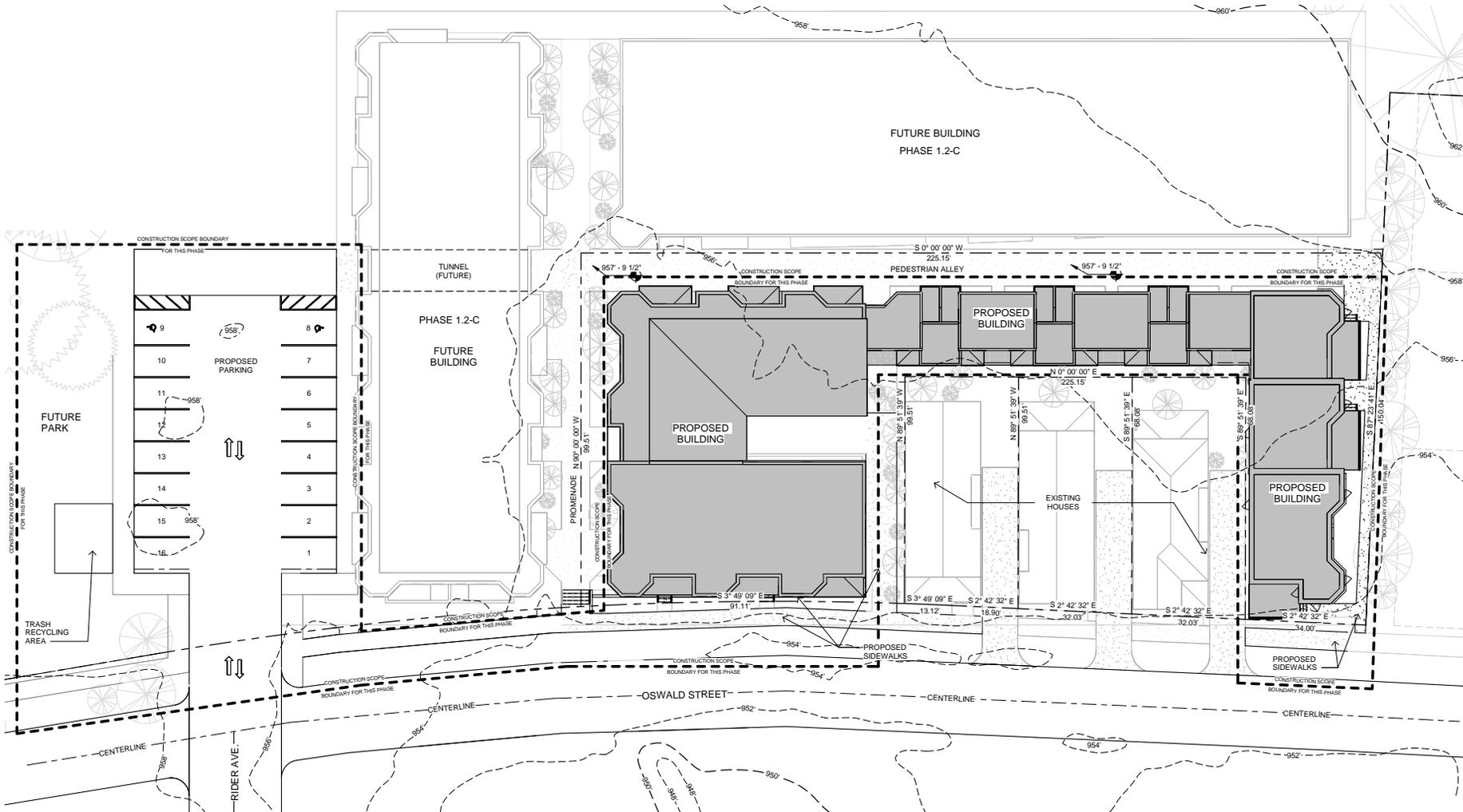
3527 OSWALD STREET  
INFILL HOUSING REVIEW



- LEGEND**
1. 2-STORIES ALONG THE PROMENADE WITH PENTHOUSE LOFTS AND GROUND FLOOR UNITS WITH PRIVATE ENTRANCES
  2. 2-STORY ALLEY LOFTS W/ PRIVATE ENTRANCES AND BALCONIES
  3. 2-STORY ROWHOUSES WITH SEMI-PRIVATE PATH TO OSWALD ST.
  4. PROMENADE
  5. ALLEY
  6. TUNNEL

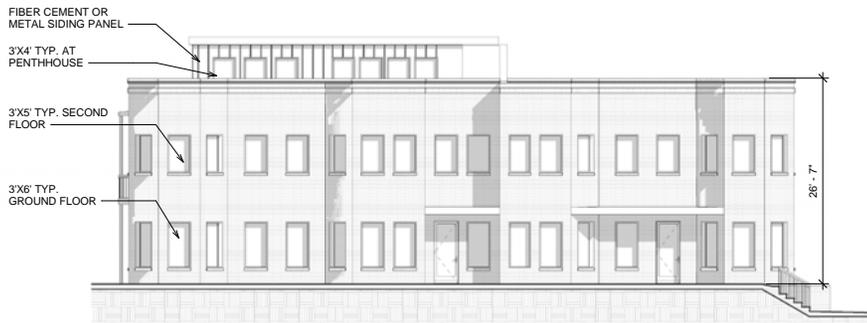
- MASTER PLAN LEGEND**
1. SINGLE FAMILY DETACHED (BUILT)
  2. MULTI-FAMILY W/ PARKING BELOW (FUTURE)
  3. MIXED-USE COMMERCIAL (FUTURE)
  4. BOUTIQUE HOTEL (FUTURE)
  5. MULTI-FAMILY RESIDENTIAL (FUTURE)
  6. PUBLIC POCKET PARK (FUTURE)
  7. MAKER SPACE (FUTURE)
  8. BREWERY (FUTURE)



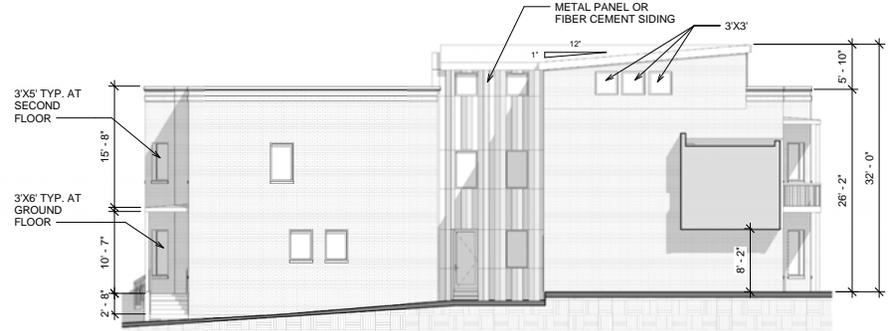


1 SITE PLAN Copy 1  
 A001 1/16" = 1'-0"

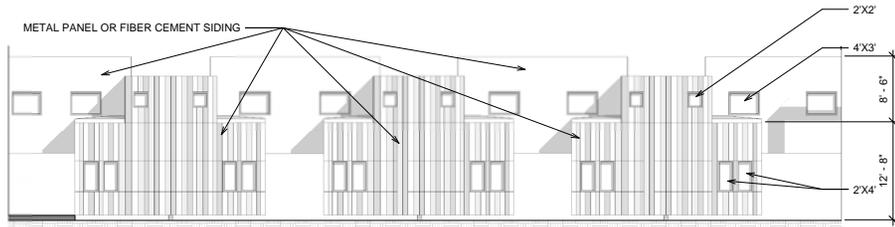




1 SOUTH ELEVATION PROMENADE  
PHASE 1.2-A  
A000 1/8" = 1'-0"

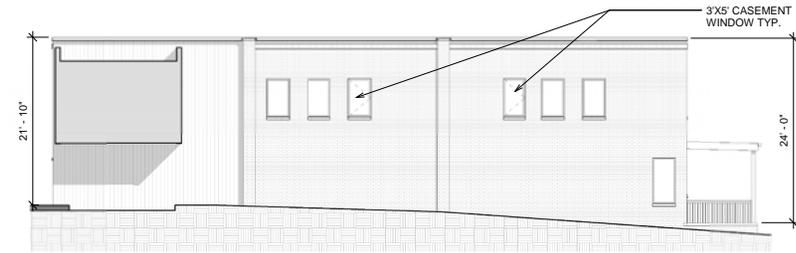


2 NORTH ELEVATION PROMENADE  
PHASE 1.2-A  
A000 1/8" = 1'-0"



3 EAST ELEVATION OSWALD STREET  
PHASE 1.2-A  
A000 1/8" = 1'-0"





1 SOUTH ELEVATION PHASE 1.2-A  
A2003 1/8" = 1'-0"



2 NORTHERN ELEVATION PHASE 1.2-A  
A2003 1/8" = 1'-0"



3 WEST ELEVATION ALLEY PHASE 1.2-A  
A2003 1/8" = 1'-0"

A Certificate of Appropriateness is hereby granted for the following property by the Knoxville Design Review Board:

**Property Address:** 3527 Oswald St.

**Parcel ID:** 69 N E 00401

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**District:** Oakwood/Lincoln Park Infill Housing Overlay District

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**Owner:** Arlington Downs Partnership, LLC

**Applicant:** Logan Higgins Heyoh Design and Development

**Contractor:** N/A

**Engineer:** N/A

**Architect:** N/A

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**Type of Work:** Level III

New Primary Structure

**Description of Work:**

New multi-family building, part of a broader mixed-use development on an approximately 8-acre property in Lincoln Park which previously held manufacturing facilities and other industrial uses. The applicant rezoned the property from I-H (Heavy Industrial) to I-MU (Industrial Mixed-Use) to allow for compatible commercial, single-family residential, and multi-family residential uses. Three single-family houses have been constructed on the property, fronting Oswald Street (8-B-21-IH).

The new building is C-shaped and two- and three-stories tall, featuring a flat roof, an exterior clad in brick and metal panel or fiber cement siding, and a brick-clad foundation. The C-shaped building wraps around the three single-family houses, with one two-story (with a third-story penthouse element) massing on the north side of the houses, a two-story massing on the west side of the houses, and a two-story massing with a recessed third-story penthouse on the south side of the houses.

The north and south massings are predominately clad in brick veneer, featuring a series of projecting bays. Windows are evenly spaced along the facades (east elevations), aligned on first and second stories, using single-light metal casements with flat brick arches and sills. Similar patterns wrap around the north and south elevations. Full-light entry doors are accessed via concrete steps. The south massing features two second-story balconies towards the left side of the façade. On the south massing, a smaller third-story penthouse is recessed from the primary roofline, featuring a flat roof and a series of single-light windows.

The center massing is clad in metal panel or fiber cement siding, with pass-throughs to access future adjacent pedestrian alley, featuring glass block adjacent to lower-level pass-throughs. Windows on the center massing are 2' by 4' and 4' by 3' metal casement and fixed windows.

The site plan includes walkways from doors to the street. Parking is planned for shared parking lots located south and west of the buildings.

The application includes phased construction for the north, center, and south massings.

The application was postponed at the August 2023 meeting. Revisions to the design include minor modifications to the building footprint and revisions to setbacks (including moving the buildings closer to the street, to align with the single-family houses). The roofline of the third-story penthouse has been modified. Window placement on secondary elevations has been modified.

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**Action:** Approved With Conditions

**Details of Action:** None noted.

**Conditions/Reasons:** APPROVE Certificate 8-G-23-IH, subject to the following conditions:

- 1) Final site plan and parking areas to meet City Engineering standards;
- 2) Any parking areas fronting Oswald Street to receive landscaping and screening to meet design guidelines;
- 3) Final drawings to meet standards of the I-MU zone and principal use standards for multi-family dwellings;
- 4) Specifications for final materials to be submitted to staff for approval.

**Action By:** Lindsay Crockett

**Action Date:** 2/21/2024

**COA Expiration Date (3 years):** 2/20/2027

**Please note:** The action of the Design Review Board may be appealed to Knoxville-Knox County Planning within 15 days of DRB decision. If Knoxville-Knox County Planning does not receive any appeals of the decision by , the applicant can proceed with obtaining permits.

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## **This Certificate Is Not A Building Permit**

This approval is not a substitute for any other reviews or permits that might be required. This Certificate is not final or enforceable until the expiration or exhaustion of all rights of appeal. Any change in the work described on this Certificate requires additional review and approval by the Knoxville Design Review Board.

For permit requirements, contact City of Knoxville Development Services: 865-215-2992 or 865-215-2991.